

Application No. SDP15-00004 (Site Development Permit)

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STAFF REPORT

I. APPLICATION INFORMATION

<u>Applications:</u>	Project No. PRJ14-00023 Site Development Permit: SDP15-00004
<u>Project name:</u>	Riva Townhomes
<u>Staff Contact:</u>	Amy Tarce, Senior Planner Development Services Department. 425-837-3097 amyt@issaquahwa.gov
<u>Applicant:</u>	Stacia Bloom Core Design 14711 NE 29 th Place, Ste. 101 Bellevue, WA 98007
<u>Owner:</u>	Gateway Smallwood LLC 14824 NE 95 th St. Redmond, WA 98052
<u>Request:</u>	Site Development Permit approval for 36 townhouses on approx. 8.39 acres. The site includes approximately 6.2 acres of critical areas, including a fish-bearing stream, and wetlands, and associated buffers for Tibbetts Creek and the wetlands, reducing the developable area to 2.19 acres. Site access is on Newport Way. A Shared Use Route (publicly-accessible multi-use trail) is provided along Newport Way and at the west boundary of the property. Private open spaces are distributed throughout the site. The majority of parking for residents are provided in individual garages, with small surface parking lots at two ends of the site.
<u>Location:</u>	The project site is located in Central Issaquah, across from the trailhead of the Cougar Mountain Regional Wildland Park (Parcel No. 202406-9115; see Attachment 1, Site Vicinity Map).
<u>Existing Land Use:</u>	The project site is currently vacant and undeveloped.
<u>Adjacent Uses (see Figure 2, Zoning Designation and Existing Land Use):</u>	
North:	Multi-family (Sammamish Pointe Condominiums and Gateway Ph. 1 & 2)
South & West:	Cougar Mountain Regional Wildland Park, Single-family Residential
East:	Tibbetts Creek and associated wetlands, Commercial/Light Industrial within the Rowley Development Agreement area

Zoning: VR, Village Residential

Comprehensive Plan:

Land Use: "Multi-family Residential"

Subarea: "Central Issaquah"

District: "Western Gateway"

II. RECOMMENDATION

Based upon the application, submitted plans, listed Attachments, and rationale contained in this Staff Report, the Administration recommends that the Development Commission approve the Site Development Permit for Riva Townhomes, SDP15-00004, with conditions.

III. SITE DEVELOPMENT PERMIT LEVEL OF REVIEW

Based on Table 4.3A, Levels of Review, in the CIDDS, this project requires a Level 3 Site Development Permit review. The process steps for a Level 3 review are outlined in Table 3.8-1.

Approval Criteria

The purpose of the Site Development Permit (SDP) is to obtain planning level approval from the Development Commission with the confidence that the project meets the standards and guidelines contained in the Central Issaquah Plan and the Central Issaquah Development and Design Standards (CIDDS), and, where appropriate, City or other applicable Code, prior to the preparation of construction documents.

The decision shall be made using applicable approval criteria including but not limited to:

- A. Consistency with the Comprehensive Plan and Central Issaquah Plan;
- B. Compliance with all applicable codes, rules, regulations, and policies; and
- C. Meets all the requirements of the Central Issaquah Development and Design Standards.

IV. PUBLIC COMMENTS

The City received comments from 4 citizens (one sent multiple emails on different dates) regarding this proposal (see Attachments 9 - 12). Staff responses are included with the public comments.

V. BACKGROUND

The Riva Townhomes site is located at the western section of town, in what was formerly an agricultural area. The site is currently bookended by two more urban developments – a multi-family residential development to the north, and the retail, commercial and light industrial uses to the southeast, in what is more commonly referred to as the Rowley Properties (See Fig. 1, *Zoning and Existing Land Use*). The site is predominantly comprised of critical areas and a critical areas report was submitted at the pre-application review to ensure that existing wetlands and streams on site were accurately delineated and mitigation measures are identified early in the process. The City conducted a peer review of the critical areas and the on-site streams and majority of the delineated wetland have been verified. The pre-application review was held on August 3, 2015. The City's Rivers and Streams Board also had the opportunity to review the proposed project, and commented on the Shared Use Route location in the wetland and long-term maintenance of the wetland buffer (see Attachment 13).

City of Issaquah Capital Project: Anti-Aircraft Creek Culvert

The proposed Riva site plan accommodates a new City culvert for Anti-Aircraft Creek. A 20-foot easement for this new Anti-Aircraft Creek culvert has been integrated into the landscape and open space elements of the Riva project, designed as a Through Block Passage.

The City of Issaquah Public Works Department received a FEMA grant to rectify a flooding issue across the Riva property, due to the existing Anti-Aircraft Creek culvert. The project involves replacing the existing culvert with a new culvert that will be re-aligned to cross Newport Way at its natural drainage path, which would move the Creek from its current location, a portion of which runs under Newport Way at Oakcrest Drive and continues along the northern perimeter of the Riva project site, to approximately 200 feet south. The culvert project will not only alleviate the flooding problem for the neighborhoods along Newport Way, but also provide fish passage. The City obtained a 20-foot wide easement on the Riva property and will be constructing this new culvert in summer 2016. A section of new stream channel would be created to pass under the Riva property as a bottomless culvert, and continue as an enhanced 21 lineal feet of existing stream channel prior to connecting to the existing stream channel at the west edge of the wetland on the Riva property. The proposal will not extend into or directly impact the wetland. For more details about the Anti-Aircraft Creek culvert project, and environmental analysis that may relate to the Riva property, please refer to Attachment 8, Anti-Aircraft Creek SEPA Mitigated Determination of Non-Significance.

VI. SEPA REVIEW

SEPA environmental review is concurrently being conducted with the Site Development Permit review. SEPA is done early in the permit process and is required to be completed before the Site Development Permit (SDP) decision. Staff has determined that environmental impacts will require mitigation. A Draft Mitigated Determination of Nonsignificance (MDNS) was issued on March 3, 2016. A 21-day combined comment/appeal period was established beginning on March 3, 2016 and ending on March 24, 2016. See Attachment 3 for the Riva SDP SEPA MDNS.

[Condition 1] ***The applicant shall comply with the Mitigation Measures set forth by the Mitigated Determination of Nonsignificance.***

The Mitigated Determination of Nonsignificance (MDNS) is based on the SEPA environmental checklist dated October 19, 2015 and other reports listed in the Notes. SEPA mitigation measures shall be deemed conditions of the approval of the licensing decision pursuant to Chapter 18.10 of the Issaquah Land Use Code. All conditions are based on policies adopted by reference in the Land Use Code and the Central Issaquah Development and Design Standards. The issued SEPA MDNS and SEPA Checklist are provided as Attachment 3 of this Staff Report.

VII. DEVELOPMENT STANDARDS AND REGULATIONS

This chapter of the Staff Report is meant to provide the rationale that served as the basis for the recommendation for the approval of the SDP, including the conditions of approval. In addition to the recommended conditions in this chapter, there are mitigation requirements for environmental impacts identified in the SEPA review for this project and construction conditions meant to address specific CIDD standards that are more appropriately reviewed during the construction permit review of projects. Please see Attachment 3 for SEPA environmental impacts mitigation requirements.

Only those goals and standards that apply to the SDP application are discussed in this report. A completed CIDD Checklist is provided as part of this Staff Report to document how the project fully complied with the CIDD and includes a comprehensive staff analysis for this project.

CENTRAL ISSAQUAH PLAN and CENTRAL ISSAQUAH DEVELOPMENT AND DESIGN STANDARDS

The following summarizes compliance, or where appropriate, the basis for the recommended Land Use and Construction Conditions for SDP15-00004, Riva Townhomes. Detailed analysis of project compliance to the Central Issaquah Development and Design Standards can be found in the CIDDS Checklist (see Attachment 2). The CIDDS Checklist staff comments are based on the Plan Drawings dated October 16, 2015 (Attachment 14), the Revised Site Plan dated Feb. 26, 2016 (Attachment 6), and Revised Fire Access Plan (Attachment 7). Many CIDDS standards can only be reviewed for compliance at the construction permit review phase. These items are marked with an “X” in the Design Checklist, under the column heading “Constn. Review” A mark in this box indicates that a standard will be substantially or fully reviewed with construction permits, since elements reviewed for compliance during land use permit review are almost always also reviewed during construction permit review as well. Additionally, the Staff comments column contain construction conditions, which shall be fulfilled by the Applicant at construction permit review. The approval of the SDP with the conditions of approval does not preclude further staff requirements during construction permits review of the project to ensure compliance with the CIDDS.

Chapter 1: Purpose and Applicability

The purpose of the Central Issaquah Plan and Development and Design Standards are to provide the tools for implementing an inspiring, animated, and connected urban community where pedestrians are priority, requiring buildings and open space that are openly inter-related, designing sites that make a positive contribution to the Public Realm, attracting businesses that complement the Central Issaquah vision, and creating a place where people of all income levels and diversities are drawn to live, work, and play.

Applicability: The subject site is located within the Central Issaquah subarea of the City. New development and redevelopment activities, such as the proposed multi-family development, are subject to the Central Issaquah Development and Design Standards. The Applicant and the City have worked collaboratively on the design of this project to meet the design standards of the Central Issaquah Plan.

Administrative Adjustment of Standards (AAS)

Unless expressly identified, approval of this SDP application does not modify any City or Central Issaquah Plan standards, which are in conflict with the elements of the SDP plan or application. Modification of the standards or guidelines requires an explicit approval in the Notice of Decision for this application or a separate Administrative Adjustment of Standards as allowed under Chapter 1.0.E (Administrative Adjustment of Standards Flexibility).

Administrative Adjustment of Standards are requested by the Applicant for:
Circulation Facilities (Chapters 6 and 12)

- Primary Through Block Passage for Units 25 – 33 (AAS16-00005)
- Parkway standards for Newport Way (AAS16-00006)

AASs are Level 2, administrative review with provision for the public to provide comments. The AASs for the Riva Townhomes are concurrently being reviewed and Staff is soliciting input from the public. Decisions for the AAS will be finalized prior to the Development Commission taking action on this SDP application.

Chapter 2: Definitions Specific to Central Issaquah Plan

Chapter 2 contains definitions for terms used throughout the Central Issaquah Plan. These are additive to the definitions in the Land Use Code. Capitalized words in this staff report are defined terms in Chapter 2.0.

Chapter 3: Procedures

Chapter 3 provides for the procedures of processing permits within the Central Issaquah Plan. Because the total site contains 3 or more acres, it is a Level 3 Review (see (Table 4.3A) in which the Development Commission is the decision maker. The City and applicant chose to not hold an optional Community Conference.

Table 3.8-1 of this Chapter requires that the Level 3 Review include: Early Coordination and Collaboration, Pre-Application Meeting, Complete Application Determination, Notice of Application, SEPA Determination, Public Hearing, Notice of Decision and provisions for Appeals and Permit Extension.

The Applicant and City Staff have collaborated extensively since the pre-application review to identify issues of compliance with the Central Issaquah Development and Design Standards and resolve these issues prior to the public hearing. The public has been provided with opportunities for early review and comment by providing the project documents on the City's website, from the time of the Pre-application review. Members of the community attended the Rivers & Streams Board meeting and provided comments also. (The meeting minutes are available at the public at the Permit Center upon request.)

Below is the project schedule following the prescribed Level 3 Review process. Some actions will occur in the future e.g. Second Public Hearing, Notice of Decision, and Appeals if one is filed.

Pre-application Meeting: **August 3, 2015**

Determination of Complete Application: **November 9, 2015**

Rivers and Streams Board meeting: **December 15, 2015**

Notice of SEPA Determination issued: **March 3, 2016** (21-day comment and appeal period begins)

Development Commission Public Hearing, part 1: **March 9, 2016**

Final Determination for SEPA: **March 24, 2016** (comment and appeal period ends for SEPA)

Development Commission Decision (Public Hearing): **TBD**

Public Notices

The Notice of Application included notices to: 1) parties of record, 2) adjacent property owners, 3) the City's website, and 4) property posting.

- A Notice of Application was posted on the City's website and mailed to adjacent property owners on **November 25, 2015**.
- Property posting with a 4' x 4' project identification sign was placed on the site on **November 25, 2015**.
- A Notice of Public Hearing was mailed to properties within 300 feet of the project on **February 26, 2016**. A Legal Notice in the Issaquah Press was published on **February 25, 2016** of the Development Commission's Public Hearing scheduled on **March 9, 2016**. Per the IMC 18.04.180.C, legal notices are required to be provided at least 10 days before the meeting/hearing.
- Notice of the Development Commission Public Hearing was also placed on the City's web site and on the project identification sign on the site.
- A Notice of Decision of the Site Development Permit, when issued, will be emailed to all parties of record and an appeal process will be provided as governed by IMC 18.04.250.

Chapter 4: Zoning Districts, Uses and Standards Summary

The intent of chapter 4 is to establish zoning districts to allow for a livable, sustainable, mixed use, urban community; balance environmental concerns with development pressures; and to ensure the health, welfare and safety of those who work, live and play in Central Issaquah.

The zoning of the property is VR, Village Residential and multi-family residential is a permitted use. The Intent of the Village Residential is to establish and preserve areas for moderate density residential uses and compatible commercial uses. The project is providing medium density residential, at approximately 12 units/developable acre. The proposed 0.76 F.A.R. meets the requirement for the VR, Village Residential zone (see District Standards Table below).

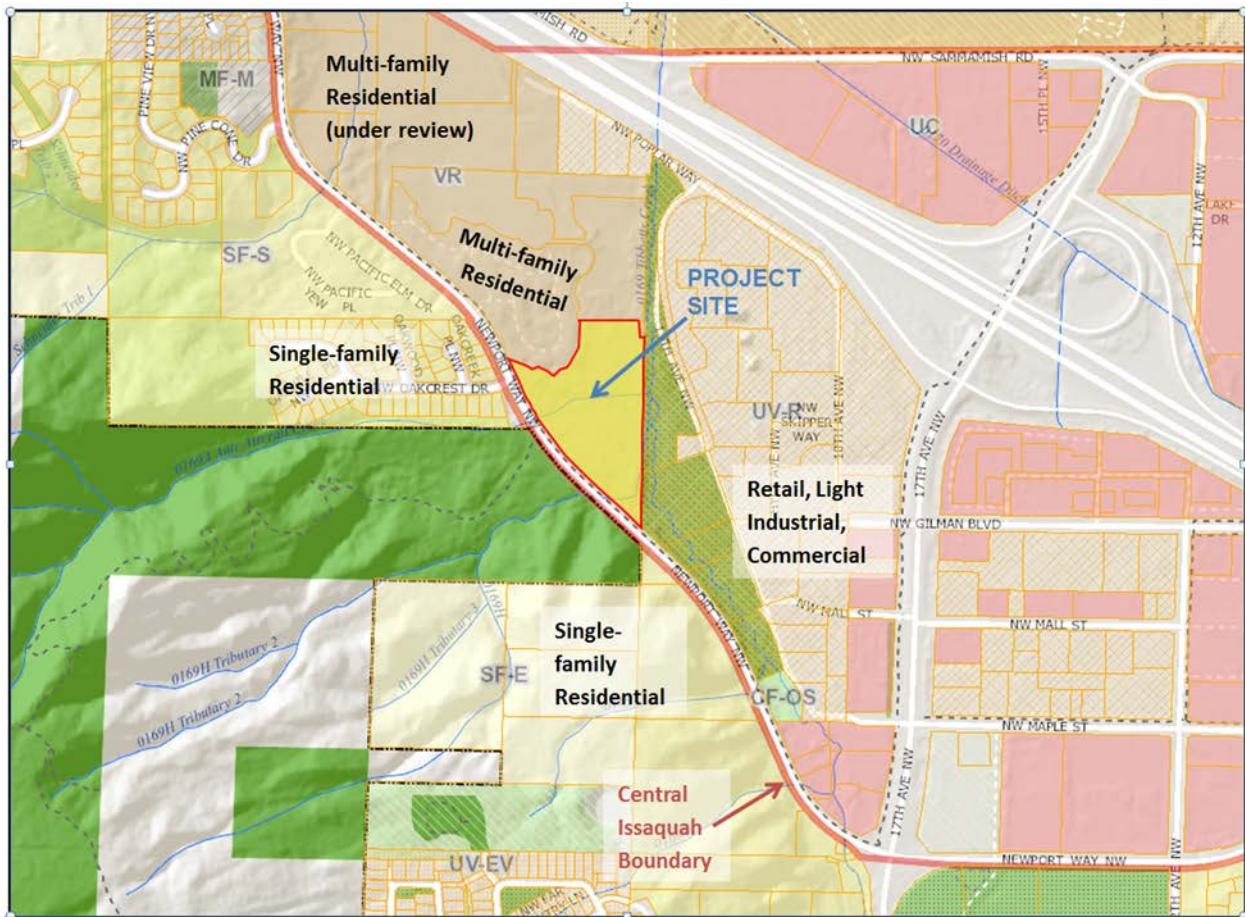


Fig. 1. Zoning and Existing Land Use

Level of Review (Table 4.3A)

See comments in Chapter 3: Procedures, above.

Permitted Land Uses

According to Table 4.3B Permitted Land Uses, a multi-family development with 5 or more units is permitted in the VR, Village Residential zone of Central Issaquah.

District Standards

Table 4.4 is the District Standards Table. Applicable sections to this table are:

<u>STANDARD</u>	<u>ALLOWED/REQUIRED</u>	<u>PROPOSED</u>
Floor Area Ratio – Base:	Minimum of 0.75 up to 1.25 (without bonus density)	0.76Riva Townhomes
Height – Base:	48 feet	35 feet, measured from finished grade
Setbacks – side and rear:	0 feet	Front setback, 4 - 6 feet from Newport Way; side and rear setbacks vary
Setbacks - Build to Line:	0-15 feet maximum	Buildings sit 6 feet from the back of the sidewalk
Impervious Surface:	90% maximum	Approximately 16.6%

Chapter 5: Density Bonus Program – not proposed or required

CIRCULATION Development and Design Standards (Chap. 6 and 12)

Design and Development Standards covering the same subject (i.e. circulation, community space, parking, landscape) are paired together even though the chapters are not sequential.

Chapter 6: Circulation Facilities Development Standards

Chapter 6 provides the appropriate standards to establish design, configuration, and performance of all Circulation Facilities that serve this project including non-motorized routes. The proposed Riva Townhomes complies with the CIDDs, with conditions. Detailed analysis of project compliance to Chapter 6 can be found in the Design Checklist.

6.1 Intent

The intent of this Chapter is to create a comprehensive Circulation Facility network that:

- Prioritizes nonmotorized users over motorized uses that are safe and convenient.
- Contributes to the Public Realm through well-designed and inviting Movement Zones.
- Provides a variety of facilities that accommodates the multiple functions that occur such as connectivity, recreation, passive use, informal gathering and stormwater.

6.2 General Standards

Block Length (6.2.A)

The requirements for pedestrian Circulation Facilities for every 300 feet length of a block is meant to ensure that there are multiple routes for pedestrians to access the site without traversing uncomfortable long distances. For measuring block length, critical areas are not included. The Riva project site has a large stream and wetland buffer at the southern end of the Newport Way frontage. Hence, the block length for the Riva project is measured from the buffer line to the northern boundary of the site, and measures 720 feet. Two Primary Through Block Passages, placed approximately 170 feet apart, and a Secondary Through Block Passage, placed between the Shared Use Route and the Primary Through Block Passage, break up the length of the block (see Fig. 2 below).

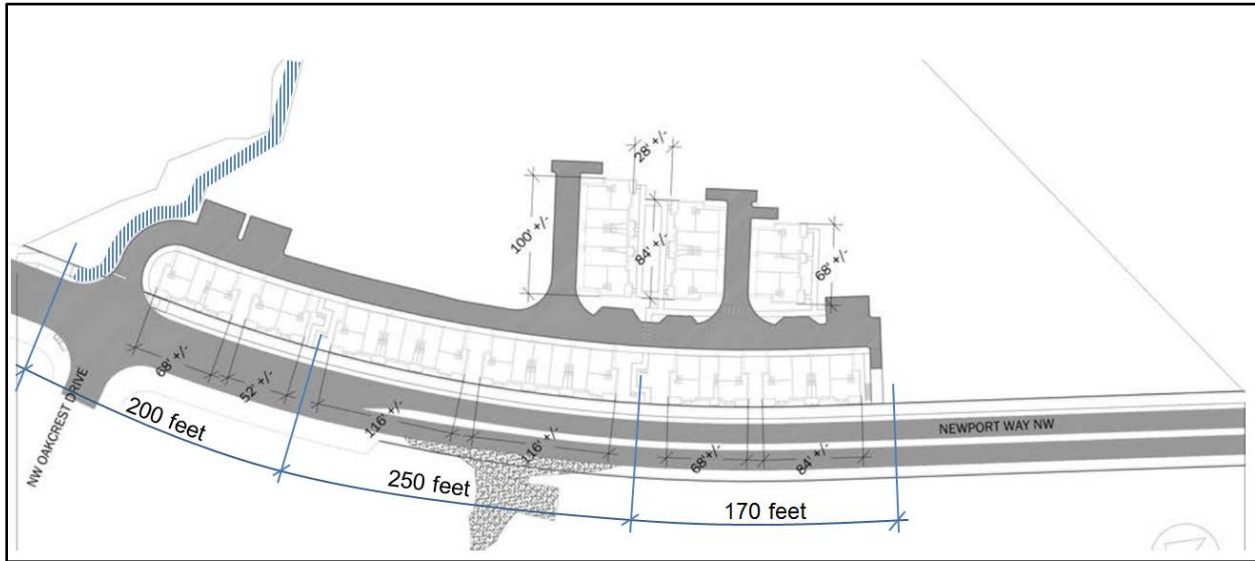


Fig. 2. Block Length showing Distance of Pedestrian Breaks

Existing and New Circulation Facilities (6.2.B)

The following describes each Circulation Facility type (Section 6.4) proposed for the Riva Townhomes and discusses how they are designed to comply with the Circulation Facility standards set forth in Section 6.4. Existing conditions and proposed frontage improvements for Newport Way are also discussed, and recommended approval conditions are identified. Street lighting is discussed under Chapter 17; additional street landscapes are discussed under Chapters 10 and 16 of the CIDD checklist.

There are three types of Circulation Facilities proposed for the Riva Townhomes project.

- A. Parkway: Newport Way
- B. Primary Through Block Passage
- C. Shared Use Route – An Off-street Shared Use Route and an existing Shared Use Route along Newport Way

Required Circulation Facilities per CIDD Fig. 6A and CIDD 12.5.A (See Fig. 5)

Table 1. Required Circulation Facilities

PROPOSED CIRCULATION FACILITY	CLASSIFICATION STANDARD	Staff Analysis
Newport Way	Parkway	Complies, with AAS
10-foot Bike-Pedestrian Trail	Shared Use Route	Complies
Walkways to interior units	Primary Through Block Passage	Complies, with AAS and conditions

1. Parkways

Parkways are scenic arterials designated to move relatively high traffic volumes at medium speeds. Newport Way NW is specified to provide street improvements including 2 travel lanes at 11 feet each, 2 bicycle lanes at 5 feet each, a center median at 12 feet, and a center turn lane at 12 feet. To keep traffic moving efficiently, longer block lengths are desired and driveways are limited. The Movement Zone (the area between the outer curb edge and the building façade dedicated to pedestrian traffic) includes landscape planters at 6' width, sidewalks at 6' width, and street lighting.

Newport Way:

Newport Way is classified as a Parkway for its entire length, according to Fig. 6A. In comparison, Figure 7B, Significant Community Spaces, show that there is an Existing Shared Use Route on Newport Way, from the western end of Newport Way within City limits to SR 900. The Parkway standards did not account for the incorporation of the existing Shared Use Route.

A. Existing Conditions

The primary vehicular access to the site will be from Newport Way NW which has a single travel and bike lane in each direction (see Figure 3). Newport Way is part of the Mountain-to-Sound Greenway corridor. The Mountains-to-Sound Greenway map shows the regional bike route going along Newport Way NW southerly of I-90 and the City has shown a Shared Use Route along this stretch of roadway to recognize the vision for this regional bike trail. A temporary regional trail pedestrian pathway on the easterly side of Newport Way NW is separated from the vehicular travel lanes by slotted curbs. Raised sidewalks generally do not exist on the Cougar Mountain side of Newport Way NW and are incomplete on the Riva side. There are currently no landscape strips or street trees on either side of Newport Way NW; therefore, the street does not meet the recently adopted CIDD Standards Parkway standard.



Fig. 3. Existing conditions along Newport Way, with the Riva site, visible on the right.

B. Required Frontage Improvements for Newport Way

The Newport Way half-street improvement required for this project includes the multi-use regional trail facility in lieu of the required sidewalk. Transitions to the existing facilities are shown to commence outside the frontage boundaries, according to City requirements (see Sheet SDP-04 and Channelization Improvements Sheets by TENW in Attachment 14).

An Administrative Adjustment of the Parkway Standards for sections of Newport Way west of SR 900 is applied to the frontage improvements for the Riva project. The AAS is for the reduction of the travel lane from 11 feet to 10 feet. All other variations shown below are required improvements per the Transportation Impact Analysis for the project, and consistent with the reduction of the speed limit on Newport Way to 30 mph. Due to this adjustment of standards, the center line of Newport Way will shift west and the pavement will be expanded. The Applicant will be required to improve the full width of the travel lane on Newport Way, provide curbs and gutters at both sides of Newport Way and relocate the existing 5-foot bike lanes that are part of the Mountains-to-Sound Greenway regional bike trail. In effect, the existing shoulder on the west side of Newport Way will be taken up by the 5-foot

bike lane (see Sheet SDP-04, Offsite Improvements of Attachment 14). This shoulder is currently used as a temporary pedestrian access to the Cougar Mountain Regional Wildland Park (see Fig.4 below)

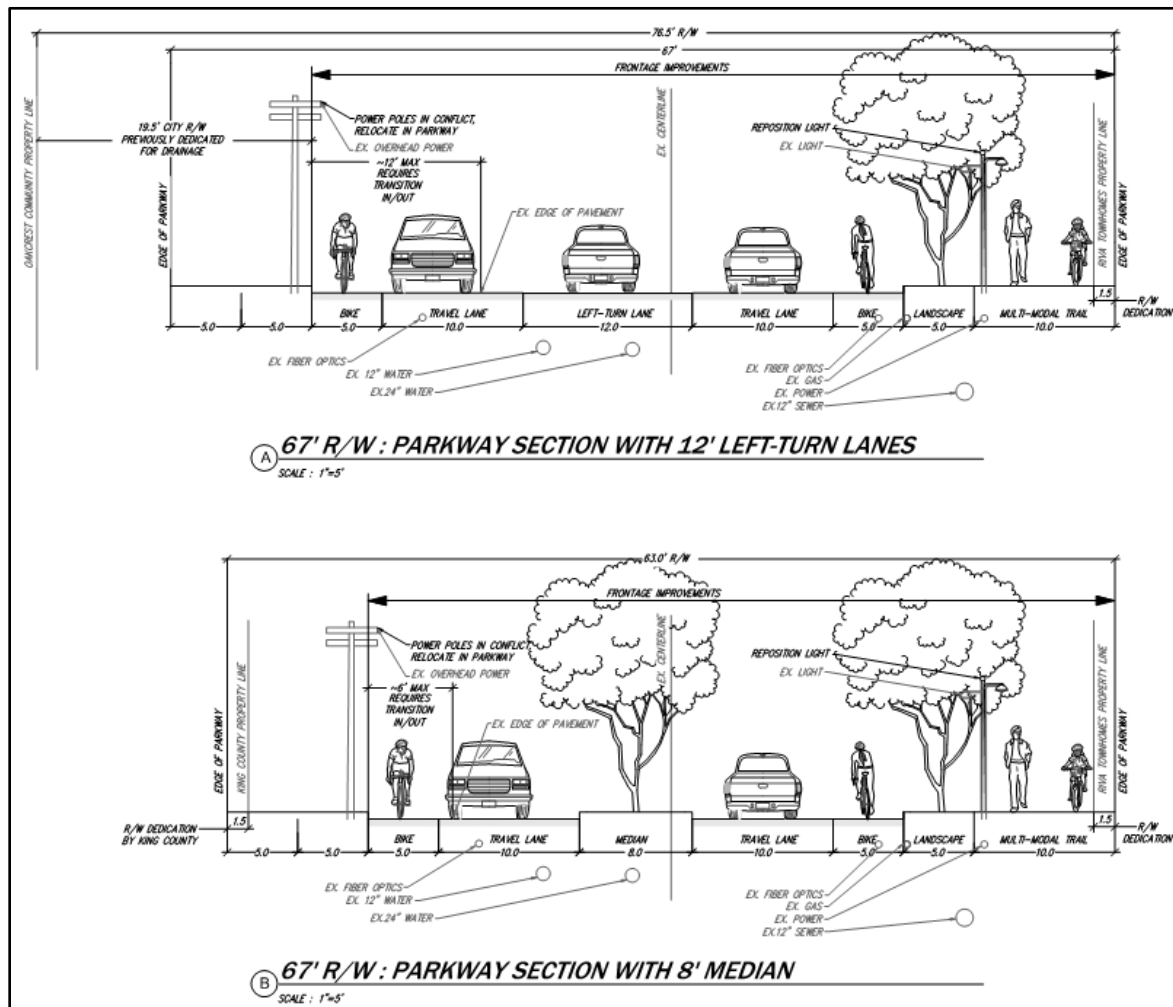


Fig. 4. Newport Way Frontage Improvements for Riva

[Condition 2] **Newport Way frontage improvements shall include:**

- driveway curb cut raised to match the grade of the 10-foot sidewalk elevation and to extend 20 feet at a flat grade. The subgrade and paving shall meet the City's Street Standards T-06 specifications to accommodate heavy truck loads;**
- driveway width of 22', with a stripe to delineate two 11-foot wide lanes. A mountable curb is required to meet any fire turning radii. The maximum driveway slope shall be 15%;**
- Shared Use Trail traversing the driveway entrance to match the grade of the Shared Use Trail running parallel to Newport Way at the point of connection. Unobstructed maintenance access to the stormfilter vaults located adjacent to the Shared Use Route shall be provided;**

- d. Existing power poles on the south side of Newport Way relocated to meet the road side safety clear zone, as defined in the City of Issaquah Street Standards;
- e. Existing pedestrian-scale street lighting at east side of Newport Way relocated to the new landscape planter strip and meet ADA clearance requirements and Chapter 17 light level requirements;
- f. Undergrounding power lines. The Applicant has two options:
 - 1. The power is undergrounded from an existing power pole that currently provides power to Sammamish Pointe Condominiums, or
 - 2. The Applicant undergrounds the power line from an existing pole across Newport Way NW. Given the Riva project will be required to move the poles on the opposite side of Newport Way that conflict with the road improvements, there may be an opportunity to utilize one of the poles that is being moved, and the undergrounding across Newport Way can be done in conjunction with all required work on the right-of-way associated with the Riva frontage improvements.

2. Through Block Passage:

The Primary Through Block Passages serve multiple purposes, including providing a circulation facility on which some buildings front if they do not face a street, breaking up the large neighborhood blocks into walkable lengths, providing major pedestrian linkages to the various community spaces on site, and creating additional open spaces between the residential buildings. They are required where block lengths exceed 300 feet and a motorized circulation facility is not required or feasible. They are an integral part of the Green Necklace, helping to ensure easy connection points from one street to another and serving as gathering spaces.

For the Riva project, three through block passages are required in conjunction with the requirement to break up the length of the block (see Figure 2 and Table 1, *Required Circulation Facilities*)

- A. Primary Through Block Passage: The interior townhouse units that do not have primary facades facing Newport Way will be accessed from a Primary Through Block Passage, per CIDDs 14.4.B.3.

A Primary Through Block Passage between Buildings 17 and 18 shows a 5-foot wide walkway that meanders to adapt to the site grade, and continues to the rear half of the site as two sidewalks serving primary pedestrian access to Buildings 25 to 33 (See Fig. 5). For the section of the Primary Through Block Passage serving the buildings 25 to 33, the split sidewalk configuration is appropriate even though it does not meet the standards because the location of the two 5-foot sidewalks provides convenient access to the building entries. An Administrative Adjustment of Standards is required to provide 2 5-foot wide sidewalks instead of the CIDDs prescribed 10-foot sidewalk. Units 34 to 36 are also served by a Primary Through Block Passage. The proposed sidewalk width is 5 feet and does not meet the standard 10 feet required. This Through Block Passage is disconnected from Newport Way (see Section 12.2.C discussion on Visual Cues)

B. Secondary Through Block Passage

A Secondary Through Block Passage is provided between units 5 and 6, where a 20-foot public easement for the Anti-Aircraft Creek culvert is provided. The 5-foot sidewalk meanders to work with the existing grade change of 10 feet from Newport Way to the alley in the interior of the lot (see Fig. 6)

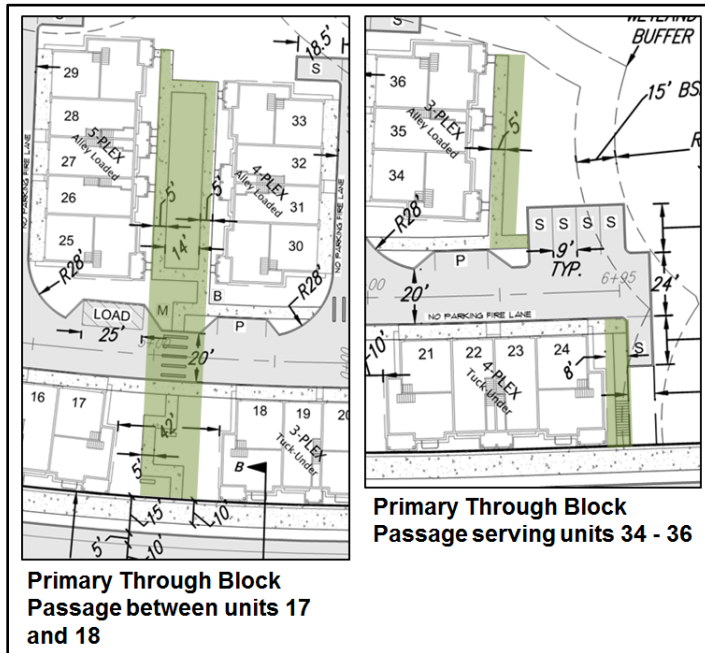


Fig. 5 Primary Through Block Passages proposed



Fig. 6 Secondary Through Block Passage proposed

[Condition 3] ***The walkway of the primary through block passage between buildings 17 and 18, between Newport Way and the alley, shall be 10 feet wide.***

[Condition 4] ***A 10-foot wide sidewalk shall be provided to serve buildings 34 to 36 and the communal open space/nature-based play area.***

3. Shared Use Route: Two Shared Use Routes are provided.

As discussed in the "Newport Way Parkway" above, a Shared Use Route will be integrated into the frontage improvements of Newport Way. As shown in Fig. 3, Existing Conditions along Newport Way, there is a 10-foot wide pedestrian/ bike path at grade with the roadway and separated by extruded curbs. This will be replaced with the Shared Use Route.

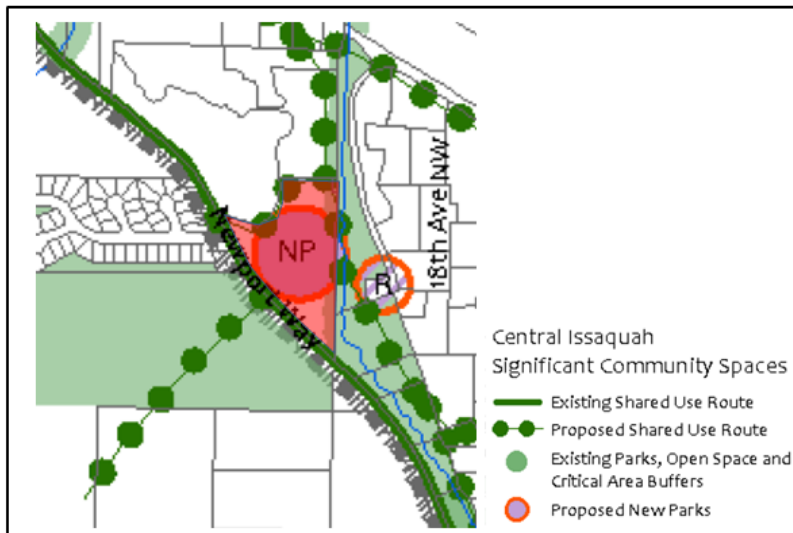


Fig. 7, Shared Use Routes per CIDDs Fig. 7B

A new Shared Use Route is also required along the north boundary of the property, as shown in Fig. 7, *Shared Use Routes per CIDDs Fig. 7B*. The Applicant has provided a site plan showing the general location of a 10-foot wide Shared Use Trail (see Attachment 6). Additional discussion of Shared Use Route is found in the Community Spaces section of this staff report.

Fire Turnaround (6.4.J)

Fire truck access, as shown on Sheet SDP-07 of Attachment 12, is not compliant and the Applicant has revised the Fire Truck Access Plan (See Attachment 7). The Fire Marshall has reviewed the revised fire truck turning radii and access paths and determined that further adjustments are required to ensure that fire trucks can turn without their buckets hitting the balconies of buildings 25 to 36.

[Condition 5] At construction permit, the site plan shall be revised to ensure the fire truck access in the alleys serving buildings 25 to 36 have adequate widths to allow fire trucks to maneuver without hitting the balconies or any parts of the buildings.

Example: Setting back the balconies or the building so that the portion of the alley with full vertical clearance is at least 22 feet wide.

Chapter 12: Circulation Design

The purpose of the Circulation Design Standards is to prioritize non-motorized users and to emphasize the role of Circulation Facilities in achieving the goal of Public Space. The following summarizes compliance, or where appropriate, the basis for Land Use or Construction Conditions. Detailed analysis of project compliance to Chapter 12 can be found in the Design Checklist.

The proposed Circulation Design for the Riva Townhomes project complies with the CIDDS at this phase of review, with conditions. Please refer to the CIDDS checklist for the comprehensive staff analysis. Items that require conditions are discussed below.

General Standards

Visual Cues (Section 12.2.C, related sections, 11.2.H)



Building entries are easily identified by the covered front porches. The site has a direct and clear circulation route, both for pedestrians and vehicles, except for the Primary Through Block Passage provided for units 34 to 36 (see Fig. 8,). Pedestrians walking along Newport Way do not have a clear and continuous path to the front doors of these units because of the small parking lot that breaks the sightline from the stairs provided at Newport Way to the sidewalk adjacent to units 34 to 36. Vehicular circulation is simple since this is a small site, with only one vehicular driveway access that leads to the garages of the residential units.

Fig. 8. Indirect route for Primary Through Block

[Condition 6] The Primary Through Block Passage serving buildings 34 to 36 shall be provided with a consistent and continuous sidewalk from Newport Way to building 36. Where the pedestrian path is interrupted by the surface parking spaces and the alley, the alley and parking area shall be designed as a pedestrian plaza (i.e., with special paving) so that it visually connects the community open space with the Primary Through Block Passage and Newport Way. The sidewalk of the Primary Through Block Passage at the section connected to Newport Way shall be 10 feet wide.

COMMUNITY SPACE Development and Design Standards (Chap. 7 and 13)

Design and Development Standards covering the same subject (i.e. circulation, community space, parking, landscape) are paired together even though the chapters are not sequential.

Chapters 7: Community Space Development Standards

Chapter 7 provides the standards to show how building design and Community Space are connected and related, that the site makes a positive contribution to the Public Realm, and that significant Community Space is located within or adjacent to the District. The proposed Circulation Design for the Riva Townhomes project complies with the CIDDs at this phase of review, with conditions. Detailed analysis of project compliance to Chapter 7 can be found in the Design Checklist.

General Description of Proposal:Green Necklace

The intent of the “Green Necklace” is to provide an array of green elements including parks, riparian corridors, tree-lined streets, multi-use trails and Through Block Passages. The existing creek and associated wetlands, which takes up over 6 acres of the site, and the Cougar Mountain Wildland Park trail are the two major Green Necklace Elements that this project take inspiration from in the site design. Additional private open spaces are introduced within the site to complement these Green Necklace elements identified in the Central Issaquah Plan. The project site is also part of the Mountains to Sound Greenway and is designed to enhance the Greenway by providing additional bike and pedestrian facilities along Newport Way. The Shared Use Route will connect the Tibbetts Creek wetland to the existing King County trailhead across Newport Way and the Mountains-to-Sound Greenway.

This project contributes to the implementation of the Green Necklace by providing:

1. Shared Use Route along Newport Way (in lieu of sidewalks at east half of Newport Way)
2. Mountains-to-Sound Greenway bike lane along Newport Way
3. Wetland and stream buffer enhancements to Tibbetts Creek and Anti-Aircraft Creek
4. Community Spaces
5. Newport Way Parkway with street trees
6. Through Block Passages serving the interior residential units

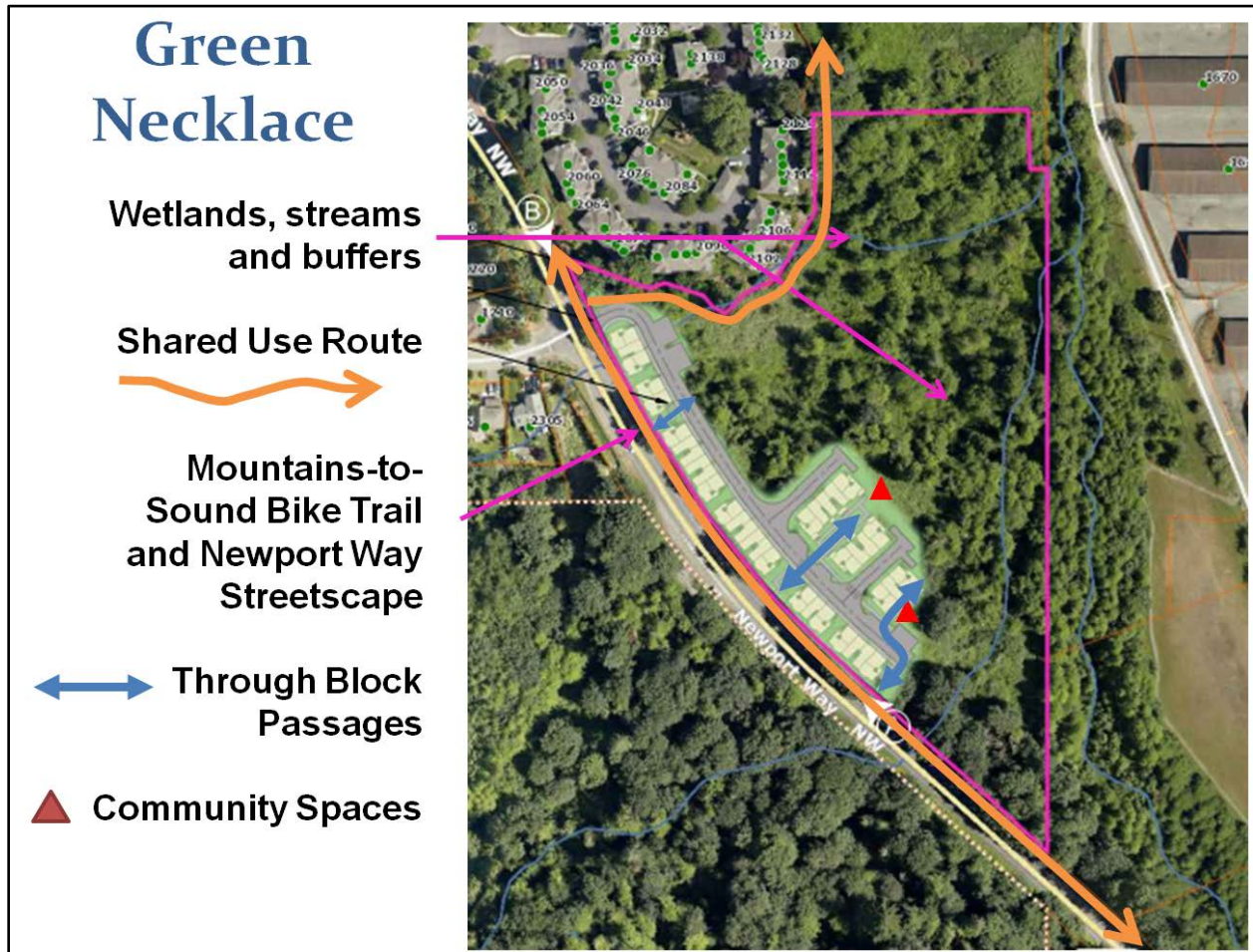


Fig. 9. Green Necklace Elements provided with the Riva project

Community Space, Residential (Section 7.3.A)

Individual private community space and an On-Site Amenity is required for residential developments. The Applicant is proposing to provide 48 s.f. of balcony space for every residential unit to meet the individual private community space requirement (see sheet A0101 of Attachment 14, Community Space Diagram, for calculations) and will provide a nature-based play area to meet the On-site Amenity requirement (labeled Open Space C on sheet L2.06). The landscape plan does not provide adequate detail about the play equipment and the size of the play area is not provided on Sheet L1.01 of the plan set.

[Condition 7] ***An On-Site Amenity, with a minimum size of 400 s.f., shall be provided. The nature-based play area may partially or completely fulfill this requirement.***

[Condition 8] ***The play equipment and landscape details of the nature-based play area shall meet the requirements in CIDDs 13.4 at a minimum.***

Significant Community Space (Section 7.4)

A Neighborhood Park and a Shared Use Route is required per Figure 7A and 7B of the CIDDS. Per 7.4.A.1, the City is selecting a Shared Use Route. This is to be a publicly-accessible community facility, integrated with the City's bike and pedestrian trail system.

[Condition 9] ***The new Shared Use Route in the Riva property shall be extended to run along the northern property line as shown in CIDDS Figure 7B, Central Issaquah Significant Community Space, and shall connect from the Shared Use Route on the east side of Newport Way and terminate at the east property line, unless the City owns the Tibbetts Creek wetland immediately north of the Riva property or the Applicant negotiates an easement with that property's owner. Then the Route shall connect to the proposed bridge over Tibbetts Creek associated with the Gateway Apartments project (SDP15-00002), instead of turning east when it reaches the northwest corner of the project site.***

[Condition 10]

If the Applicant is receiving park impact fee credits for the Shared Use Route, the Shared Use Route shall be completed and dedicated to the City prior to issuance of the first Building Permit. If the Applicant is not receiving park impact fee credits, the Shared Use Route shall be completed prior to issuance of the Certificate of Occupancy for the 24th residential unit or upon completion of 66% of the proposed units.

[Condition 11] ***Upon completion of the Shared Use Route, it shall be owned by the City of Issaquah; however, dedication of this facility may take City Council action. The property owner or Homeowner's Association (HOA), should one be formed, will have a right, but not the obligation, to maintain the Shared Use Route. If the HOA does not take the maintenance obligation, then it would be the City's obligation if the City owns the facility. If the City is not the owner, an access easement to the City will be required and maintenance responsibilities would remain with the owner or HOA.***

Parks and Recreation Mitigation and Credits (Section 7.5)

The Applicant will receive Park Impact Fees credit for constructing the Shared Use Route.

Chapter 13: Community Space Design Standards

This is a relatively small development with a significant wetland on site. The community spaces provide passive recreation in the form of outdoor seating areas and active recreation in the form of a bicycle/pedestrian multi-use trail and a nature-based play area.

[Condition 12] ***Pet waste stations shall be provided at appropriate locations, such as the outdoor community spaces, where pets are likely to be allowed.***

Integration (CIDDs 13.2.B)

The community spaces are integrated with the non-motorized circulation facilities and provide access to viewing the wetland and creek buffer areas. Primary through block passages are integrated into the Open Space A and Open Space C. Open Space C will also be designed with a nature-based play area. Buildings frame the community spaces, and residential units front Open Space A and Open Space C. Open spaces also have a direct view of Cougar Mountain across Newport Way. The open spaces can easily be accessed through the walkways that connect from the front doors of residential units and the Shared Use Route along Newport Way.

Connect with Nature (CIDDs 13.3)

The open spaces provided are integrated with the residential units and the larger Green Necklace elements that include the wetland buffers on site and the Shared Use Route along Newport Way. It is also visually connected to the Cougar Mountain trailhead across Newport Way.

PARKING Development and Design Standards (Chap. 8 and 15)

The intent of the parking chapters are to establish parking standards based on urban rather than suburban densities that support a pedestrian-friendly environment and attractive urban design. The proposed Circulation Design for the Riva Townhomes project complies with the CIDDs at this phase of review, with conditions. The following summarizes compliance, or where appropriate, the basis for Land Use or Construction Conditions. Detailed analysis of project compliance to Chapters 8 and 15 can be found in the Design Checklist.

General Description of Proposal:

Parking for the Riva Townhomes project is primarily in individual garages with additional surface parking lots or individual stalls. Additional visitor parking, above what is embedded in the standards, is not required by the CIDDs. Surface and structured parking is accessed from the alleys. One loading space is provided.

Chapter 8: Parking Standards

The proposed parking complies with Chapter 8 of the CIDDs at this phase of review. ADA parking spaces are not required per code. Bike parking for visitors and temporary use are provided at the center of the site, along the Through Block Passage serving units 25 to 33. Each unit is provided with bicycle storage space in the garage (see floor plans on sheets A-01 to A-04) Motorcycle parking is missing and required per CIDDs 8.12, A.1. A detailed analysis of parking standards can be found in the CIDDs checklist (See Attachment 2)

[Condition 13] ***Provide at least one motorcycle parking space.***

[Condition 14] ***Provide at least 2 temporary bike parking near the Shared Use Route proposed at the northern perimeter of the site.***

[Condition 15] ***For head-in surface parking abutting the open spaces and wetland buffers, reduce the amount of impervious area by reducing the length of stall to 16.5 feet long with a 2-foot overhang into the landscaped area.***

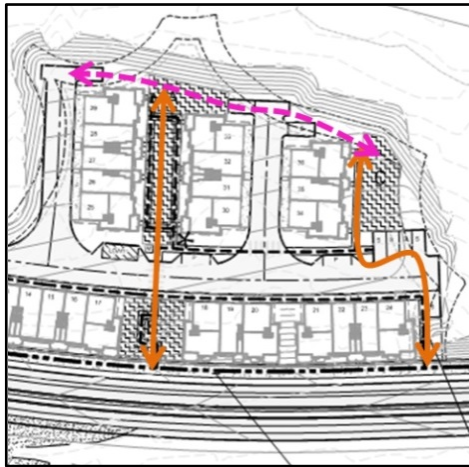
Chapter 15: Parking Design Standards

The purpose of the Parking Design Standards is to use a more urban approach to parking to support a pedestrian friendly, small scale, mixed use environment and contribute to the Public Realm. The site complies with the design standards. The following summarizes compliance, or where appropriate, the basis for Land Use or Construction Conditions.

Location (Section 15.2.A)

Surface parking proposed next to open spaces and in the building setback line cuts off the open spaces from the pedestrian circulation and residential units (see sheet L2.06). 1) The single parking space next to unit 24 could be better used as a walkway to connect the exterior stairs at Newport Way to Open Space C along the edge of the stream buffer.

[Condition 16] ***Remove single surface parking space next to unit 24 and connect the exterior stairs to Open Space C and primary through block passage serving units 34 to 36.***



Pedestrian-friendly Techniques (Section 15.2.E)

The proposed surface parking spaces are clustered at two ends of the site, and single spaces are interspersed along the Building Setback Line (BSBL on site plans) of the wetland buffer. In the Primary Through Block Passage discussion, it was noted that the parking at the south end of the site will be required to be designed as a pedestrian plaza. The visitor parking spaces are located within the BSBL. A pedestrian path along the BSBL between the two open spaces (see dashed arrow in Fig. 10) would not only provide connectivity to the two open spaces but also provide a walkway from these individual parking spaces to the front doors of the units.

Fig. 10, Pedestrian Link to Parking Spaces

[Condition 17] ***Provide a hard surface pedestrian connection from the individual, northern parking stalls to the proposed walkways within the community open spaces.***

Multi-functional (Section 15.2.F)

There are no surplus parking spaces proposed; however, there are several car turn-around stubs located at the end of the alleys that can potentially be used as nature look-outs.



Fig. 11, Opportunities for Nature Look-outs

[Condition 18] ***The driveway aisles within the BSBL that are used for vehicle turn-around shall be designed as viewing areas.***

LANDSCAPE Development and Design Standards (Chap. 10 and 16)

The proposed Landscape Design for the Riva Townhomes project complies with the CIDDS at this phase of review, with conditions. The following summarizes compliance, or where appropriate, the basis for Land Use or Construction Conditions. Detailed analysis of project compliance to Chapters 10 and 16 can be found in the Design Checklist.

Chapter 10: Landscape Development Standards

Intent: Chapter 10 provides landscaping standards with the intent to draw nature into the developing urban community, adding green elements to soften the urban form, and create a livable, verdant, attractive Public Realm that restores nature and human activity and contributes to the success and establishment of the Green Necklace.

The proposed Riva Townhomes project complies with the Landscape Development Standards at this phase of review. Detailed analysis of project compliance to Chapter 10 can be found in the Design Checklist.

Minimum Tree Density/Tree Removal/Tree Retention (Sec 10.10)

This section requires a minimum tree density retained and replanted in the Developable Site Area at a ratio of 4 significant trees, or their equivalent per code, per 5,000 square feet. At least 375 trees, at 2-inch caliper, are required as mitigation. Street trees planted on Newport Way do not count towards the site's required mitigation. Trees proposed on site will be used for mitigation.

TREE DENSITY	
Total number of onsite significant trees and landmark trees	57
Total number of significant and landmark trees proposed for removal	57
Developable Site Area	95, 378 s.f.
CIDDS 10.10 Min density 4 significant trees/5000 sq. ft.	76 trees
Mitigation: At 6 in. per significant tree, a total dbh of 456 inches of new trees is required. This equates to 229 new trees, 2 in. dbh at planting.	229 new trees

Tree Retention Requirements (10.13)

Tree retention requires that 25% of the tree caliper be retained. The plan meets the required tree caliper retention (see sheets L1.01 to L1.04) The majority of trees to be retained are located in the critical areas and the perimeter of the site. The significant trees proposed for retention is included in the Tree Density calculation.

[Condition 19] ***A total of 76 significant trees, or its equivalent in dbh, shall be provided as replacement trees. If the site cannot accommodate all replacement trees, the Applicant shall be required to pay a fee-in-lieu or plant trees off-site at a location approved by the Director of DSD. Adjustments to the number of trees which will be removed or required, will adjust the***

number of replacement trees, using a consistent methodology to that shown in the staff report.

Chapter 16: Landscape: General standards and Guidelines

The purpose of the Landscape Design Standards is to provide a variety of green elements to implement the Green Necklace, soften the built environment with landscape, integrate development with the natural environment, and use landscape as screening where necessary. Detailed analysis of project compliance to Chapter 16 can be found in the Design Checklist. The following summarizes compliance, or where appropriate, the basis for Land Use or Construction Conditions.

General Description of Proposal: Landscaping is proposed to soften the appearance of the proposed townhomes and screen areas such as parking facilities. The landscape plan incorporates a variety of native species and naturalistic planting schemes to immerse the user into a contextually specific Pacific Northwest experience. The planting palette and layout are consistent throughout the project with the exception of a more intense use of natives as the landscape plan moves from Newport Way towards the critical areas. Accent plantings are incorporated into the planting palette and proposed to be arranged to add visual interest to the landscape design through the incorporation of color, seasonal character and texture variety. It is not clear where accent plantings are to be located (CIDDs 16.2.F).

Key landscape elements include the community open spaces, through block passages and shared use route. The proposed landscape plan contains a high number of ornamental and native trees. The mitigation buffer will also contain a high number of native trees. The trees are strategically located to fulfill an aesthetic function, increase the biodiversity of the site and enhance the critical areas.

Site Furnishings (CIDDs 16.2.O) As depicted sheet L1.03 – site furnishings will provide a nature-inspired modern aesthetic to the site to marry the contemporary architecture of the proposed buildings with the natural features of the site including the onsite critical areas and Cougar Mountain Regional Park across Newport Way. Pedestrian-scale lighting is not shown on the plans but will be incorporated and reviewed with construction permits.

[Condition 20] ***Pedestrian- scale pole lighting shall be provided at the designated open spaces, including the Primary Through Block Passages.***

Surface Parking Screening (CIDDs 16.2.Q)

Near the entrance/exist of the project there is an area to allow for 8 street-side perpendicular spaces which will be thoroughly screened with vegetation on all three sides of the parking area including a landscape strip integrated into the spaces to reduce possible visual impacts of the parking areas.

[Condition 21] ***Single parking spaces distributed throughout the site and the 4-space parking lot at the eastern end of the property shall be screened where it abuts the pedestrian areas and community open spaces.***

Chapter 11: Site Design

Chapter 11 establishes site design standards that orient development so that it defines the Public Realm and improves the pedestrian experience. Pedestrian and bicycle circulation needs are raised to a priority with motorized circulation priorities while ensuring that the design does function for motorized transportation. Detailed analysis of project compliance to Chapter 11 can be found in the Design Checklist.

11.2 General

Projects are required to create a strong identity for itself and the Western Gateway district of Central Issaquah. This project meets the general standards, as discussed in the CIDDS checklist staff analysis. Site design features, which are listed below, are discussed in greater detail throughout the staff report. The discussion below is meant to emphasize the most important elements of the Site Design. Standards not included here are discussed more fully in the CIDDS checklist.

- A. **Integrating the development into the Green Necklace** through the enhancement of the Tibbetts Creek's and wetland buffers, extension of the Newport Way Parkway treatment, and addition of a new multi-use trail to link the regional Mountains-to-Sound Greenway to the new Shared Use Route provided by the Gateway Apartments project;
- B. **Circulation Priorities:** Supports pedestrian and bicycle use by providing attractive pedestrian and bike facilities, bike storage in every building, and multiple pedestrian routes to the Community Spaces.
- C. **Sense of Place (11.2.C)**
The sense of place can be best characterized by the dominant feature that occupies two-thirds of the property – the natural areas consisting of the wetlands and the two streams on site. The Cougar Mountain hillside across Newport Way completes the nature-dominated character of the site. Within the development, the interior open spaces allow for informal gathering, as well as contemplative enjoyment of the natural areas from the look-out. Additional look-out areas will be required and conditioned for this project (see condition #17). The Shared Use Route along the northern end of the property also provides more active recreation while immersing a bicyclist or jogger in nature.

The architecture style is contemporary but fits within the context of the Newport Way Western Gateway. Fig. 12 above shows the building façades visible from Newport Way. The Northwest character is expressed in the contemporary design by the rhythmic cadence of shed roofs, the balconies and large windows of the living units that allow views of the natural area, the use of native plant palette and integrating the buildings along Newport Way into the natural grade. Consistent with its contemporary style, the building exterior does not use any natural materials. The proposed exterior colors consist primarily of different grey tones with an accent color applied to the front door of units. The accent colors will be red or a yellowish brown shade (labeled "Toasty" in the Material and Color Board). The proposed materials are fiber cement board applied in a lapsiding fashion and "stucco" made of the same material (see typical façade elevations below). A material/color board will be shared at the Development Commission meeting sharing examples of proposed materials and colors for the buildings.

[Condition 22] ***The building elevations shall be further refined to incorporate natural materials.***



Fig. 12. Architecture Character of Newport Way Facade

D. Views and Vistas (11.2.G)

The units along Newport Way will have unobstructed views of Cougar Mountain. The interior units will have views of the wetlands and Tibbets Creek natural areas. Views to the site from Newport Way are provided through the Through Block Passages, which allow unobstructed views of the wetland buffer in the interior of the site (See Fig.13 below). The Shared Use Route that runs along the northern edge of the site also allows views of the wetlands and streams in the interior of the property, and Tibbetts Creek and its associated wetlands in the adjacent property. This project will not have significant adverse impacts of neighboring properties' views of Cougar Mountain.



Fig. 13. Conceptual view from Newport Way of the Primary Through Block Passage framed by buildings and with axial view into the wetlands in the interior of the site.

Chapter 14: Buildings

Chapter 14 establishes building design standards that create a vibrant, Pedestrian Friendly, built environment through buildings designed to frame and engage the Public Realm. The proposed Riva Townhomes project complies with the Buildings standards at this phase of review, with conditions. Detailed analysis of project compliance to Chapter 14 can be found in the CIDD Checklist (see Attachment 2).

Building Mass and Design (Sec. 14.3)

Building mass and design are meant to reinforce pedestrian-friendly public spaces through the modulation of height and massing, as well as the use of architectural details to further provide interest at the street level. The Riva project consists of several groupings of townhouse units, from 2 to 6 units. The design of the buildings meets the standards for articulation, modulation and the change of building materials.

The buildings have a consistent menu of parts, including private garages, two floors of living spaces, and habitable attic spaces enclosed by a main roof (see Fig. 14 below) modulated with the use of smaller dormer and shed roofs that are lower in height. Surface relief, depth and shadows are provided for the buildings by emphasizing some bays and adding balconies on the alley side of the building (see Rear Elevation in Fig. 15 below). The roof is broken up into smaller gables and shed roofs, completing the overall effect of creating a rhythm of porch canopies, bays and shed roofs to break up the mass of the building. The roof mass is also mitigated by the cascading smaller roofs, which is most apparent for the side elevations, (see Fig. 15 below).



Fig. 14. Typical Façade visible from Newport Way



Fig.15. Typical Side Elevation and Rear Elevation for Newport Way buildings

CIDDs 14.3.A.9 require special treatment of building corners when the building is visible from two intersecting Circulation Facilities. This standard would apply to units 9, 10, 17, 18 and 25, where Through Block Passages

intersect Newport Way. Based on the elevations provided, which are all typical elevations, no special corner treatments have been proposed.

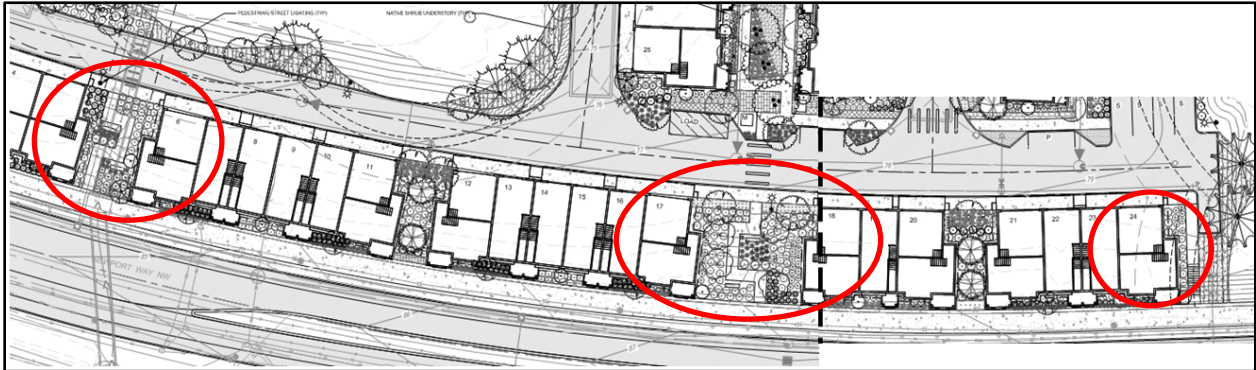


Fig. 16. Buildings that require special treatment at corners

[Condition 23] *Provide additional architectural details to enhance the more visible corners of units along Newport Way that frame the required Through Block Passages.*

Ground Level Details (Sec. 14.4)

Architecture and landscaping features are required to enhance pedestrian experience at the ground level, using techniques such as large window coverage, active interior spaces clearly visible from the public areas, enhanced landscaping, special paving, pedestrian scaled lighting and weather protection.

All residential units have their primary entrance facing directly at a Circulation Facility, either Newport Way or a Primary Through Block Passage. All primary entries are adorned with raised front porches that serve to provide a transition from semi-public to semi-private space. Note: There are two versions of elevation drawings for the “AL” building type in this staff report. The Applicant is proposing a revised elevation (see Attachment 6, Updated Site Plan and Elevations) that shows vertical windows for most of the units at the ground level.) The building design has some notable ground-level design features that warrant mentioning, including:

- 1) Grade-elevation changes from building entry to the garage face result in a more interesting side elevation but the interior units that are on relatively level grade result in blank walls that need to be mitigated. The blank walls of the end units visible from pedestrian paths will be mitigated by planting trees and a variety of plants as shown in Fig. 17 below to meet the Ground Level Details requirement.
- 2) Basement bonus room requires window wells that will be visible from the Through Block Passages and other pedestrian paths (see Fig. 18). These will be required to meet Building Code egress requirements and may have to provide a safety railing or other safety measures per the Residential Building Code.



Fig. 17. Interior buildings, Type 'AL' and proposed planting to mitigate blank walls

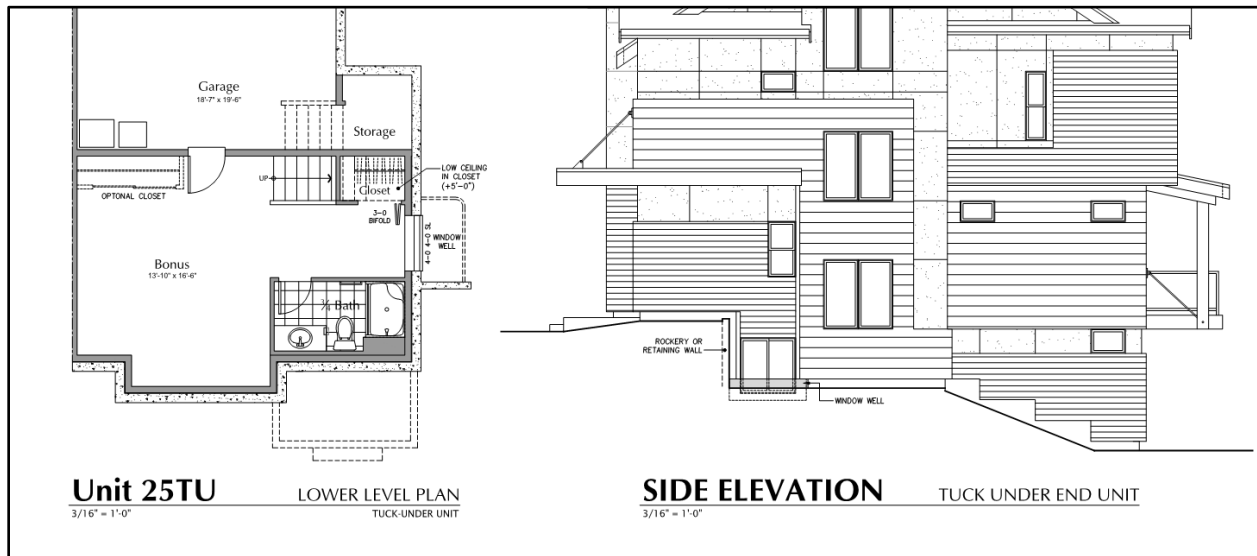


Fig. 18. Window wells occur at end units of all buildings along Newport Way

[Condition 24] ***Window wells shall be integrated to the landscape design and screened from pedestrian walkways with a decorative railing or low shrubs.***

Chapter 17: Lighting

Chapter 17 provides the standards for lighting. Sheets A2.05 and A2.06 of Attachment 14 show the proposed lighting located at the end of the alleys (see Pedestrian/Drive Lighting shown below). No lighting is shown for any other locations.

Exterior light fixtures should not just be functional and utilitarian but used as an element in creating the urban public realm. Two types of decorative pole lighting will be used for the project. Pedestrian-scale street lighting currently exists on Newport Way. These will be retained but relocated with the adjustment to the location of sidewalks and drive lane widths for Newport Way. Lighting photometrics will be required at construction permit review and lighting details will be further reviewed for compliance with lighting standards at that time. See condition for CIDDS 16.2.O requiring lighting for Community Spaces and Through Block Passages.

VII. ADDITIONAL REVIEW: OTHER CITY STANDARDS, OUTSIDE AGENCIES

Grading

A slope analysis was conducted by the applicant, which determined that the existing slopes meet the City's limited exemptions for steep slope hazards as defined in IMC 18.10.580. Mitigated Determination of Nonsignificance SEPA decision has been issued and together with other concerns addressed this analysis and found the findings consistent with the aforementioned City Code.

Utilities

Storm:

The City has adopted the 2009 King County Surface Water Drainage Manual together with the City of Issaquah 2011 Addendum, both of which together identify the requirements for the storm water conveyance, detention, and treatment systems. Preliminary plans and reports indicate that the project will comply with the above standards and requirements.

[Condition 25] ***A public storm drain is required along the easterly margin of the site and shall be constructed to City standards including a public storm drain easement.***

Sewer:

The Applicant is proposing to connect to a public sewer system that the property is party to as part of an existing sewer easement agreement with the Rowley property, the Mull properties, and the Sammamish Point Condominiums, as recorded in King County records, under recording number 9605151208. The proposed design is based upon Section 3. Limitations on Grant of Easement. If the connection is not allowed per the agreement, the project may be served by a public sewer system on Newport Way, but must be pumped. Should it be determined that the capacity of the existing sanitary sewer line located within the sewer easement area must be increased to accommodate development, cost sharing is defined in the agreement.

[Condition 26] ***The applicant shall demonstrate that there is adequate capacity per the sewer easement agreement.***

Water:

A looped water system is proposed, subject to the completion of offsite improvements on the Gateway Apartment site. If the Issaquah Gateway Apartments section is not constructed, an onsite looped water system is still required. A fire flow analysis shall be conducted to determine if the offsite water system in Newport Way NW requires upsizing.

[Condition 27] ***The 8-inch water main shall be looped through project with two connections to the existing public 12-inch water system located along Newport Way NW, providing for fire flow consistent with City Code. A continuous 10' public water main easement is required along the alignment of the water main.***

[Condition 28] ***A fire flow analysis shall be conducted to determine if the offsite water system in Newport Way NW requires upsizing consistent with City Code.***

Review comments received from other City departments, listed below, have been incorporated into the Staff Report.

- Eastside Fire & Rescue – see Fire Truck Turnaround condition
- Public Works Engineering: Standard construction conditions were sent to Applicant under a separate cover.

VIII. Proposed Motion

Based upon the applications, submitted plans and technical reports, listed Attachments, and rationale contained in the Staff Report, the Administration recommends that the Development Commission move to:

Approve the Site Development Permit for the project known as Riva Townhomes, File No. SDP15-00004, subject to the terms and conditions of the Staff Report dated March 3, 2016, Attachments 1 thru 14, and the following conditions:

General Conditions

- A1. Any above ground and at-grade utilities will need to be located to eliminate their visual impact in buildings or underground. Locations shall be shown on the first Site Work permit (such as for roads, paving, utilities, not clearing and grading). Some options for screening may be acceptable with architecture and/or landscaping and shall be worked out prior to approval of the final landscaping plans.
- A2. Alleys: Alleys generally have vertical curbs placed at the edge of the drive aisle except where larger vehicles such as fire trucks and garbage trucks require a larger turning radius. Where this is required, alternative materials such as concrete shiners and rolled curbs will be used.
- A3. Accent plantings shall be provided at the Newport Way entrances to the primary through block passages to provide visual cues that these are the main pedestrian access into the interior of the site and that these walkways lead to the community open spaces on site.

-
- 1 The applicant shall comply with the Mitigation Measures set forth by the Mitigated Determination of Nonsignificance.
 - 2 Newport Way frontage improvements shall include:
 - a. driveway curb cut raised to match the grade of the 10-foot sidewalk elevation and to extend 20 feet at a flat grade. The subgrade and paving shall meet the City's Street Standards T-06 specifications to accommodate heavy truck loads;
 - b. driveway width of 22', with a stripe to delineate two 11-foot wide lanes. A mountable curb is required to meet any fire turning radii. The maximum driveway slope shall be 15%;
 - c. Shared Use Trail traversing the driveway entrance to match the grade of the Shared Use Trail running parallel to Newport Way at the point of connection. Unobstructed maintenance access to the stormfilter vaults located adjacent to the Shared Use Route shall be provided;
 - d. Existing power poles on the south side of Newport Way relocated to meet the road side safety clear zone, as defined in the City of Issaquah Street Standards;

-
- e. Existing pedestrian-scale street lighting at east side of Newport Way relocated to the new landscape planter strip and meet ADA clearance requirements and Chapter 17 light level requirements;
 - f. Undergrounding power lines. The Applicant has two options:
 - The power is undergrounded from an existing power pole that currently provides power to Sammamish Pointe Condominiums, or
 - The Applicant undergrounds the power line from an existing pole across Newport Way NW. Given the Riva project will be required to move the poles on the opposite side of Newport Way that conflict with the road improvements, there may be an opportunity to utilize one of the poles that is being moved, and the undergrounding across Newport Way can be done in conjunction with all required work on the right-of-way associated with the Riva frontage improvements.
 - 3 The walkway of the primary through block passage between buildings 17 and 18, between Newport Way and the alley, shall be 10 feet wide.
 - 4 A 10-foot wide sidewalk shall be provided to serve buildings 34 to 36 and the communal open space/nature-based play area.
 - 5 At construction permit, the site plan shall be revised to ensure the fire truck access in the alleys serving buildings 25 to 36 have adequate widths to allow fire trucks to maneuver without hitting the balconies or any parts of the buildings. Example: Setting back the balconies or the building so that the portion of the alley with full vertical clearance is at least 22 feet wide.
 - 6 The Primary Through Block Passage serving buildings 34 to 36 shall be provided with a consistent and continuous sidewalk from Newport Way to building 36. Where the pedestrian path is interrupted by the surface parking spaces and the alley, the alley and parking area shall be designed as a pedestrian plaza (i.e., with special paving) so that it visually connects the community open space with the primary through block passage and Newport Way. The sidewalk of the primary through block passage at the section connected to Newport Way shall be 10 feet wide.
 - 7 An On-Site Amenity, with a minimum size of 400 s.f., shall be provided. The nature-based play area may partially or completely fulfill this requirement.
 - 8 The play equipment and landscape details of the nature-based play area shall meet the requirements in CIDDs 13.4 at a minimum.
 - 9 The new Shared Use Route in the Riva property shall be extended to run along the northern property line as shown in CIDDs Figure 7B, Central Issaquah Significant Community Space, and shall connect from the Shared Use Route on the east side of Newport Way and terminate at the east property line, unless the City owns the Tibbetts Creek wetland immediately north of the Riva

property or the Applicant negotiates an easement with that property's owner. Then the Route shall connect to the proposed bridge over Tibbetts Creek associated with the Gateway Apartments project (SDP15-00002), instead of turning east when it reaches the northwest corner of the project site.

- 10 If the Applicant is receiving park impact fee credits for the Shared Use Route, the Shared Use Route shall be completed and dedicated to the City prior to issuance of the first Building Permit. If the Applicant is not receiving park impact fee credits, the Shared Use Route shall be completed prior to issuance of the Certificate of Occupancy for the 24th residential unit or upon completion of 66% of the proposed units.
- 11 Upon completion of the Shared Use Route, it shall be owned by the City of Issaquah; however, dedication of this facility may take City Council action. The property owner or Homeowner's Association (HOA), should one be formed, will have a right, but not the obligation, to maintain the Shared Use Route. If the HOA does not take the maintenance obligation, then it would be the City's obligation if the City owns the facility. If the City is not the owner, an access easement to the City will be required and maintenance responsibilities would remain with the owner or HOA.
- 12 Pet waste stations shall be provided at appropriate locations, such as the outdoor community spaces, where pets are likely to be allowed.
- 13 Provide at least one motorcycle parking space.
- 14 Provide at least 2 temporary bike parking near the Shared Use Route proposed at the northern perimeter of the site.
- 15 For head-in surface parking abutting the open spaces and wetland buffers, reduce the amount of impervious area by reducing the length of stall to 16.5 feet long with a 2-foot overhang into the landscaped area.
- 16 Remove single surface parking space next to unit 24 and connect the exterior stairs to Open Space C and Primary Through Block Passage serving units 34 to 36.
- 17 Provide a hard surface pedestrian connection from the individual, northern parking stalls to the proposed walkways within the community open spaces.
- 18 The driveway aisles within the BSBL that are used for vehicle turn-around shall be designed as viewing areas.
- 19 A total of 76 significant trees, or its equivalent in dbh, shall be provided as replacement trees. If the site cannot accommodate all replacement trees, the Applicant shall be required to pay a fee-in-lieu or plant trees off-site at a location approved by the Director of DSD. Adjustments to the

number of trees which will be removed or required, will adjust the number of replacement trees, using a consistent methodology to that shown in the staff report.

- 20 Pedestrian- scale pole lighting shall be provided at the designated open spaces, including the Primary Through Block Passages.
- 21 Single parking spaces distributed throughout the site and the 4-space parking lot at the eastern end of the property shall be screened where it abuts the pedestrian areas and community open spaces.
- 22 The building elevations shall be further refined to incorporate natural materials.
- 23 Provide additional architectural details to enhance the more visible corners of units along Newport Way that frame the required Through Block Passages.
- 24 Window wells shall be integrated to the landscape design and screened from pedestrian walkways with a decorative railing or low shrubs.
- 25 A public storm drain is required along the easterly margin of the site and shall be constructed to City standards including a public storm drain easement.
- 26 The applicant shall demonstrate that there is adequate capacity per the sewer easement agreement.
- 27 The 8-inch water main shall be looped through project with two connections to the existing public 12-inch water system located along Newport Way NW, providing for fire flow consistent with City Code. A continuous 10' public water main easement is required along the alignment of the water main.
- 28 A fire flow analysis shall be conducted to determine if the offsite water system in Newport Way NW requires upsizing consistent with City Code.